

## Chapter 18.41: Off-Street Parking and Loading

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### **18.41.010 Purpose**

The specific purposes of the off-street parking and loading regulations are to:

1. Ensure that parking uses are provided for new land uses and major alterations to existing uses to meet the parking needs created by such uses.
2. Establish standards and regulations for the developer, owner, or operator of any specific use occurring outdoors or within an existing, newly constructed, or relocated building to provide well-designed, off-street parking areas.
3. Ensure that off-street parking and loading areas are designed and located to protect the public safety; minimize congestion and conflict points on travel aisles and public streets; and where appropriate, buffer surrounding land uses from their impact.

### 18.41.020 Applicability

- A. **Generally.** Parking shall be provided for each new development or building, enlargements to existing developments or buildings, or changes in use consistent with the provisions of this chapter. All parking facilities required or not, shall meet all the standards of this chapter.
- B. **Additions/Enlargements/Intensification of Use.** New parking shall be provided for additions/enlargements to an existing development or building in accordance with the following requirements. The requirements of this chapter shall apply whether the addition/enlargement or intensification occur individually or cumulatively over time.
1. If a building addition/enlargement or intensification of use necessitates an increase in the amount of physical parking spaces already existing on the site of *less than 15 percent* in order to achieve the total number of parking spaces required by Section 18.41.040, then only those additional spaces must meet the dimensional, landscape, and all other requirements of the chapter.
  2. If a building addition/enlargement or intensification of use necessitates an increase in the amount of physical parking spaces already existing on the site by an amount *greater than 15 percent*, those additional spaces must meet the dimensional, landscape, and all other requirements of this chapter *and* the existing parking area shall be modified, if necessary, to provide at least 60 square feet of landscape for each existing parking space. The existing parking-stall and aisle configurations need not be modified to meet the requirements of this section. Where determined feasible by the Director, at least 50 percent of the increased landscape shall be installed within 60 feet of the adjacent public street right-of-way. To facilitate this landscape, the Director may authorize up to a 10 percent reduction in the total number of spaces required for the development provided that the reduction does not exceed the minimum necessary to install the additional landscape. The Director also may authorize a schedule for completion of the work, not to exceed a maximum time of 3 years.
- C. **Changes in Use.** If the cost of remodeling a building with a floor area greater than 25,000 square feet to facilitate a change in use exceeds 50 percent of the pre-improvement value of said building, based on data provided by the Shasta County Assessor, the existing parking area shall be modified, if necessary, to provide at least 60 square feet of landscape for each existing parking space. To facilitate this landscape, the Director may authorize up to a 10 percent reduction in the total number of spaces and/or space dimensions. This requirement applies regardless of whether the change in use results in an increased parking requirement as determined by this chapter. The Director may authorize a schedule for completion of the work, not to exceed a maximum time of 3 years.

### 18.41.030 General Provisions

- A. **No Reduction in Off-Street Parking Spaces.** Off-street parking spaces existing as of the date of adoption of this Code and actually being used for parking in connection with the use of an existing building shall not be reduced in number or size during the entire life of such building or land use below that which would be required for a new building or use of a similar type constructed or commenced under the requirements of this Code. No property owner shall sublease, sub-rent, or otherwise encumber the off-street parking spaces required by this section.
- B. **Fractional Spaces.** If the number of off-street parking spaces required by this chapter contains a fraction, such number shall be rounded to the next whole number. For example, if computed requirements equal 9.3 spaces, ten (10) spaces shall be required.
- C. **Computation of Required Parking Per Residential Use.** Residential parking for multiple-family and condominium developments is based on the number of bedrooms. Any rooms having the potential of being a bedroom and meeting the standard of the Uniform Building Code as a bedroom shall be counted as a bedroom for purposes determining off-street parking requirements.
- D. **Uses Not Mentioned.** In case of a use for which off-street parking requirements are not specified at all in this chapter, the requirements for the most nearly similar use (as determined by the Director) for which off-street parking requirements are specified shall apply.
- E. **Mixed Uses.** When two (2) or more uses are located on the same lot or parcel of land the number of off-street parking spaces required shall be the sum total of the requirements of the various individual uses computed separately in accordance with this chapter.
- F. **Off-Street Parking Assessment District.** Areas within an established off-street parking assessment district may be exempted by resolution of the City Council from the off-street parking requirements of this chapter or allowed a modification of those requirements. Such exemption or modifications may be allowed if a finding is made that the off-street parking facilities provided in conjunction with the assessment district will serve the off-street parking needs of the area as well or better.
- G. **Joint Parking.** Where parties wish to cooperatively establish and operate parking facilities where one use generates parking demands primarily during hours when the remaining use(s) is not in operation or where adjacent uses generate joint/redundant trips, a reduction of up to fifty percent (50%) of the required parking may be approved by site development permit by the Board of Administrative Review (BAR). Such approvals shall require:
  - 1. The submission of satisfactory statements by the parties providing such facilities and the parties such facilities are to serve, describing the nature of the uses and times when such uses operate so as to indicate the lack of conflict between such uses;

2. Such documents or commitments as may be deemed necessary in each particular case to ensure provision and maintenance of the required off-street parking spaces.

- H. **Reductions for Unique Parking Demand.** Recognizing that some nonresidential uses are unique in their off-street parking demands, upon an appropriate application and an adequate showing, the Director, by zoning exception, may reduce the off-street parking requirement up to ten percent (10%) of that required. To ensure that additional parking space areas will be available for development if the use were subsequently intensified, parking spaces waived by such procedures may be required to remain either in landscaped area or in unimproved open-space as determined by the Development Services Director.
- I. **Bus Stop/Transportation Facility Credit.** With approval of a Site Development Permit, required parking for commercial and multiple-family residential uses with four (4) or more units may be reduced by up to five percent (5%) where a bus stop is located within four-hundred feet (400') of the use or up to ten percent (10%) where a bus transfer facility is located within four-hundred feet (400') of the use. Such a reduction may be allowed provided a written finding is made which indicates that the land use is conducive to the use of public transportation. A bus transfer facility has a minimum of four (4) bus routes converging in a central location, with the facility having passenger amenities, which include benches, trash receptacles, information kiosk, and covered waiting areas.
- J. **Off-Site, Off-Street Parking.** For a nonresidential use, the Director may approve a Site Development Permit authorizing up to thirty percent (30%) of the required off-street parking to be met at an off-site location provided that such facilities: (1) are located within four-hundred feet of the proposed use, (2) are improved to the standards of this chapter applicable at the time the parking lot was constructed, (3) are not already being utilized under the requirements of this Code, and (4) do not require pedestrians to cross through an existing or planned four-lane arterial street, unless separated pedestrian access is provided. The Director shall require documents, agreements, commitments, and other evidence as deemed necessary in each particular case before such findings are made.
- K. **Temporary Alternative Use of Off-Street Parking.** The temporary use of off-street parking spaces for outdoor display/sales of merchandise in conjunction with a lawful business on the premises is allowed provided that:
1. The temporary use shall not extend longer than four (4) consecutive calendar days at any one time.
  2. The non-parking use or uses shall not consist of more than sixteen (16) days in any one calendar year.
  3. The use does not utilize more than 10 percent (10%) of the off-street parking spaces required under this code.

4. The use does not create a traffic hazard.

A site development permit approved by the Development Services Director is required to exceed these standards.

**L. Exception for Downtown Core District, Mixed-Use Projects, and Historical and/or Architecturally Significant Districts.**

1. The off-street parking requirements listed in Section 18.41.040 shall not apply in the Downtown Core District as defined by the Downtown Specific Plan. If a property owner chooses to install parking, the other requirements of this chapter shall apply. However, no more than fifty percent of the parking spaces that would otherwise be required may be installed without first obtaining a Site Development Permit.
2. For projects in the Parkview Avenue Mixed-Use Corridor or other mixed-use area adopted in conjunction with a neighborhood revitalization plan or similar document, typical on-site parking requirements may be reduced under a Mixed-Use Development Plan approved per Chapter 18.54. Where a reduction in on-site parking is granted, opportunity for on-street and/or off-site parking shall be identified within a reasonable distance to serve the needs of the project, and parking for the residential component, equaling at least one space per unit, must still be provided on-site.
3. In order to preserve the unique historical and/or architectural character of certain areas of the City, the off-street parking requirements listed in Section 18.41.040 shall not apply in the following circumstances and geographical areas:
  - a. The conversion/use of existing structures on the west side of Oregon Street between Shasta Street and Yuba Street, and on the west side of Pine Street between Eureka Way and Trinity street, to uses allowed by right or by discretionary permit by the base zoning district. Such exclusion shall not apply to enlargements of buildings exceeding twenty percent (20%) of the existing floor area in which case off-street parking shall be provided for the entire building in accordance with the requirements of the Downtown Specific Plan.

**M. Parking Space and Aisle Dimension Reductions.** Because of circumstances unique to a property, such as size, shape, topography, location of easements, or desirable trees, the Director may authorize by zoning exception up to a ten percent reduction in space or aisle-width dimensions or number of on-site parking spaces required for commercial and multiple-family residential uses with four or more units. Such exception may be allowed only when it is proven that the reduction will not result in a traffic hazard or impact the necessary parking for the use.

**N. Off-Street Parking for Senior Citizen and Disabled Persons Projects.** Off-street parking serving disabled persons' developments or developments serving low- and moderate-income senior citizens may be reduced to one-half parking space per dwelling

unit where a site development permit or use permit is issued specifically for a senior citizen or disabled person project.

- O. **Recreational Vehicle (RV) Parking for Affordable Housing Projects.** The requirements for providing RV parking for multiple-family units may be waived by zoning exception, provided the development is an affordable-housing project as defined by the State Health and Safety Code and provided the area for RV parking is set aside in landscape to be converted to RV parking in the event the use converts to standard multiple-family use.
  
- P. **Other Requirements.** The off-street parking requirements set forth in this chapter are nonexclusive and shall not be deemed to repeal, modify, or otherwise affect such off-street parking requirements or regulations as may be set forth in other parts of this code or the General Plan, provided and excepting, however, that if the off-street parking requirements of the chapter with respect to any property, building, or use should be different from those applicable to such property, building, or use under or by virtue of any provision or provisions of other parts of this Code, then in that event, the more stringent off-street parking requirements shall apply..

**18.41.040 Off-Street Parking Spaces Required**

All covered and uncovered off-street parking spaces required by Schedule 18.41.040-A shall be improved and maintained in accordance with the requirements of this chapter.

Unless otherwise approved pursuant to Sections 18.41.030(G) or (J), the required parking spaces shall be located on the same building site as the use or building they are intended to serve. On-street parking within public or private streets or parking in a driveway shall not be used to satisfy the off-street parking requirement except for planned developments and condominiums or as exempted by the provisions of a specific plan, neighborhood plan, or resolution governing a specific area adopted by the City Council.

**Schedule 18.41.040-A: Parking Spaces Required**

**OFF-STREET PARKING**

USE	SPACE REQUIREMENTS
Caretakers' quarters	2 spaces per unit
Animal services	1 space per 350 square feet of gross floor area.
Automotive parts stores	1 space per 350 square feet of gross floor area.
Automotive repair service	1 space per 350 square feet of gross floor area.
Automotive sales	1 space per 2,000 square feet of interior and/or exterior sales, display, or storage area up to a total 20 spaces, plus 1 space.
Bank	1 space per 300 square feet of gross floor area.
Bar	1 parking space for each 3 seats, plus 1 space for each 40 square feet of dance floor or assembly area.

Redding Zoning Ordinance

USE	SPACE REQUIREMENTS
Boardinghouse	1 parking space per each bedroom plus 2 additional parking spaces overall.
Boat sales	1 space per 2,000 square feet of interior and/or exterior sales, display, or storage area up to a total 20 spaces, plus 1 space.
Bowling alley	4 parking space per each bowling lane plus 1 parking space for each 200 square feet of gross floor area devoted to accessory uses.
Bulk retail store	1 parking space for each 600 square feet of gross floor area.
Colleges and trade schools, Public or Private	1 parking space for every 15 square feet in the classroom seating.
Call Centers	1 space for each employee.
Card room	1 parking space for each 2 seats in the play area plus 1 space per 3 seats in any bar or dinner area.
Condominium (residential)	1.5 spaces per on-bedroom or efficiency units, 2 parking spaces per unit for 2 or more bedroom units, plus 1 guest parking for each 5 units and 1 recreational-vehicle parking space for each 5 units. For 2-bedroom condominiums, 1 required space shall be covered. For 3 or more bedroom condominiums, there shall be 2 covered spaces.
Day care center/preschool	1 parking space for each 10 students plus 1 space for each teacher/employee plus 1 passenger loading space.
Dry cleaners (non-institutional)	1 space for each 400 square feet of gross floor area.
Dwelling groups	Parking shall be provided in accordance with the requirements of each individual unit type. (See Single Family, Two-Family Dwelling, and/or Multiple Family, as applicable)
Elderly housing	.5 space per 1-bedroom unit; .66 space per 2-bedroom unit.
Furniture and large appliance store	1 parking space per 750 square feet of gross floor area.
Golf driving range	1 parking space for each driving tee.
Health club and fitness studio	1 parking space for each 125 square feet of gross floor area for the 5,000 square feet of building area. 1 space for each 300 square feet of building area. 1 space for each 300 square feet of gross floor area shall be provided for that building area which exceeds 5,000 square feet. 1 parking space shall be provided for each 350 square feet of enclosed swimming pool and deck area.
Hospital	1.5 parking spaces per bed and 1 space for every 225 square feet of gross floor area dedicated to outpatient or office use.
Hotel, Residential	1 parking space for each unit, plus 2 spaces.

USE	SPACE REQUIREMENTS
Industry	
a. Assembly (the combining of manufactured parts to make a completed product)	1 parking space per 750 square feet of gross floor area.
b. Manufacturing	1 parking space per 1,000 square feet of gross floor area.
c. Warehouse/distribution	1 parking space for each 1,250 square feet of gross floor area for buildings between -0- and 20, 000 square feet in area; 1 parking space for each 2,000 square feet of gross floor area for the building area between 20, 000 and 40, 000 square feet, and 1 parking space for each 4,000 square feet of gross floor area for the building area over 40, 000 square feet.
Laundry (coin operated)	1 parking space for each 250 square feet of gross floor area.
Medical office, clinic, or physical therapy services	1 parking space for each 225 square feet of gross floor area. Special requirements for provision of accessible parking may apply—see Section 18.41.170(A)(1)(c).
Ministorage (self-storage)/long-term, public storage	3 spaces plus 2 spaces for manager’s unit where individual units are accessible by vehicles; 1 space per 10,000 square feet where individual units are not accessible by vehicles.
Mobile home park	2 parking spaces per unit, 1 of which shall be covered, plus 1 guest space for each 5 units plus 1 recreational-vehicle space for each 5 units.
Mortuary	1 parking space for every 4 seats in the principle seating area plus 3 parking spaces overall , or 1 parking space for each 45 square feet in the principal seating area, whichever is the greater.
Motel/hotel	1 parking space for each motel unit plus 2 spaces. 5 percent of the required spaces shall be pull through RV spaces (48 feet by 10 feet, unless an alternative design can be proven to be as effective).
Multiple-family dwelling	1.5 parking spaces per one-bedroom unit, 1.75 parking spaces per 2-bedroom unit, 2 parking spaces per each 3-bedroom unit, plus 1 guest space for each 5 units that are provided beyond the initial 30 units, plus recreational-vehicle spaces equal to 5 percent of the required number of parking spaces. Recreational vehicle spaces shall be a minimum of 10 feet wide by 25 feet in length. Required parking in developments serving low-and moderate-income seniors may be reduced by 50 percent.
Personal services	1 parking space for each 250 square feet of gross floor area.
Planned (residential) development	2 covered spaces per unit, plus 1 guest space for each 5 units, plus 1 recreational-vehicle space for each 5 units unless approved otherwise with the Planned

USE	SPACE REQUIREMENTS
	Development Plan. Recreational vehicle spaces shall be provided in a common parking area maintained by a homeowner's association or other approved entity.
Pool and billiard room	1.5 parking spaces for each table.
Professional office	1 parking space for each 350 square feet of gross floor area.
Real estate office	1 parking space for each 250 square feet of gross floor area.
Religious facilities, social hall, club, lodge, community center, other public assembly	1 parking space for each 4 seats in the principal seating theatre area, or 1 parking space for every 40 square feet in the place of principal seating/assembly area, whichever is the greater, plus 1 passenger loading space. Each 24 inches of bench-type seating is to be considered 1 seat.
Research and development	1 space for each 250 square feet of gross floor area.
Residential care, senior and general	1 parking space for each 3 beds.
Restaurant	
a. Sit-down	1 parking space for each 3 seats. Up to 30 percent of the number of seats provided indoors may be provided outdoors without additional required parking, provided that the number of parking spaces required for the number of indoor seats is met.
b. Drive-through, fast-food, or self service	1 parking space for each 75 square feet of gross floor area. Required reservoir spaces shall count towards required parking.
c. Coffee kiosk	1 parking space per window, plus 1 space.
Retail	
a. Enclosed	1 parking space for each 300 square feet of gross leasable floor area for single-tenant buildings of 10,000 square feet of floor area or greater and 1 space for each 250 square feet of gross leasable floor are for single-tenant buildings of less than 10,000 square feet of floor area and all multi-tenant buildings. Multi-tenant buildings may be occupied by any mix of permitted uses and uses permitted with the approval of a Site Development Permit or Use Permit. (Note: Parking requirements for shopping centers approved, by Use Permit, Site Development Permit, or other discretionary approval shall be determined by the final decision making body).
b. Outdoor	1 space for each 750 square feet of outdoor display area.
c. Open lot nursery	1 space for each 1,250 square feet of outdoor display area.
Accessory dwelling unit	1 uncovered space (see Section 18.43.140.C.7. for locational requirements and exceptions).
Single-family dwelling	2 covered spaces.
Two-family dwelling	1 covered space per unit plus 1 other space per unit.

- A. **Bicycle Parking.** Projects must comply with the short-term and long-term bicycle parking requirements, as applicable, of Section 5.106.4 of the California Green Building Standards Code.
- B. **Visitor Parking.** On-street parking may be counted toward the visitor-parking requirement for planned developments or condominiums provided that the street has an 8-foot-wide parking lane that is not posted or likely to be posted prohibiting parking and that to qualify as one visitor-parking space, there must be an uninterrupted 22-foot-long space and a public sidewalk adjacent to the street.
- C. **Company Vehicles.** Commercial or industrial uses are to provide one parking space for each company vehicle which is parked on the site during normal business hours. Such space may be located within a building.
- D. **Motorcycle Parking.** Facilities with 25 or more spaces may provide motorcycle parking at a rate not exceeding 3 percent of required spaces. Such spaces shall be counted toward meeting the total number of spaces required for the development by this Chapter. Spaces delineated for motorcycle parking shall have a minimum area of four (4) feet by ten (10) feet and shall be identified as a motorcycle space by signs or pavement delineation.

**18.41.050 Drive-Up Facilities**

Drive-up facilities shall be designed to allow safe, unimpeded movement of vehicles at street access points and within the travel aisles and parking space areas. Drive-through aisles shall be a minimum of 11 feet in width with minimum interior and exterior turning radii of 15 and 30 feet, respectively, and shall be set back from the street in accordance with Section 18.41.100(B). Drive-through aisles located adjacent to a street shall be screened by a solid masonry/block wall or landscape berm with a finished earth grade a minimum of 42 inches in height.

In addition to the required off-street parking, drive-up facilities shall provide 18-foot-long stacking or vehicle reservoir spaces behind the vehicle being serviced in accordance with Schedule 18.41.050-A. See Section 18.43.080 for additional requirements for drive-up facilities.

**Schedule 18.41.050-A: Drive-Up Facilities**

Car Wash	4 reservoir spaces per pull-through rack. 1 reservoir space per self-service manual wash bay.
Coffee Kiosk with Drive-Through	4 reservoir spaces per window.
Drive-Through Bank	4 reservoir spaces for 1 teller drive-up. 3 reservoir spaces each for facilities with 2 or more teller drive-ups.
Drive-Through Pharmacy	2 reservoir spaces per window.
Drive-Through Restaurant	7 reservoir spaces per window.
Photo Drop	2 reservoir spaces per window, unless service is also available indoors, in which case only 1 reservoir space is required.
Service Station	1 reservoir space shall be located on each end of the pump aisle.

**Note:** A reservoir space is the area a vehicle occupies while waiting to be served at the service window, Service Island, or bay and is 18 feet in depth by a minimum of 11 feet in width. A reservoir space does not include the space of the vehicle being served.

### **18.41.060 General Standards for Off-Street Parking Spaces**

A. **Location and Design.** Subject to other applicable provisions of this code and other applicable ordinances, the off-street parking spaces required by this chapter may be incorporated within the main building to be served or within an accessory building, or open space may be provided therefor. All off-street parking spaces must be situated within the same lot or parcel within or upon which the main building or use is situated or conducted. Each off-street parking space shall be of a size and dimension which shall comply with the minimum criteria hereinafter required, shall be of usable shape and condition, and shall be maintained during the life of the building or use for which it is required to be provided.

Proposed off-street parking lot layouts, submitted to satisfy the requirements of this chapter, which deviate in design from the patterns provided below, must demonstrate that adequate provision is afforded for safe ingress and egress; that adequate aisle width and maneuvering area are provided for safe maneuvering of vehicles therein; and that the design provides for safe pedestrian traffic to, from, and within the subject parking area.

B. **Areas Not To Be Used for Parking or Loading.** Neither the off-street parking spaces required by the provisions of this chapter nor the off-street loading spaces so required shall, under any circumstances, be situated within any street side or front setback area required by other provisions of this title for the lot or parcel on which such parking spaces or loading spaces are located. The parking spaces for any office, commercial, or industrial use located within 50 feet of a residential district shall be set back commensurate with the front setback or corner side setback of the adjoining residential district.

C. **Ingress and Egress, Maneuvering Area, Protection of Other Property.** Each off-street parking space shall be provided with:

1. A driveway not less than the widths specified in Section 18.41.180 which shall provide ingress thereto from a public street or public way and egress therefrom to a public street or public way.
2. A maneuvering area, such as an aisle or driveway of appropriate dimension or design, which must comply with minimum criteria hereinafter required and which will provide safe and efficient means of entry or exit by automobiles and other motor vehicles thereto and therefrom.
3. Where 5 or more parking spaces are required by the provisions of this chapter and such spaces will be used by the general public or business invitees of the owner or lessee of the premises, the following requirements shall also be applied:

- a. Parking stalls shall be delineated by painted, white 4-inch lines, or other contrasting paint or other easily distinguishable material, which shall be installed and maintained thereafter to designate each parking space in a clearly identifiable manner.
- b. The off-street parking areas shall provide suitable maneuvering areas so that all vehicles parking therein may exit onto any adjoining street in a forward direction at a right angle (perpendicular) to the public street.
- c. Parking spaces that abut another parcel shall be provided with bumper guards, curb-line landscape, or wheel stops not less than 4 inches in height in such a manner as to prevent encroachment of vehicles over property lines or damage to walls or fences.

D. **Compact Parking Spaces.** For parking facilities with greater than 20 spaces, up to 10 percent of the stalls in a required off-street parking area for multiple-family, commercial, industrial, or public-assembly uses may be designed and designated as compact spaces.

#### **18.41.070 Dimensions for Covered and Open Parking Spaces**

- A. **Covered Spaces.** Each required, covered, off-street parking space shall include a minimum of 10 feet in width and 20 feet in depth of unobstructed area provided for parking purposes. The required minimum measurements may not include the exterior walls or supports of the structure, nor shall it include areas used for washers, water heaters or dryers, or stairways or steps provided that one 6-inch rise may be provided as a front-wheel stop for a car entering a stall in a forward direction. A covered, off-street parking space shall have an unobstructed back-up area of not less than 25 feet in length. Where there is landscape or a nonpublic sidewalk with a minimum width of 6 feet located contiguously to the rear of the back-up area, the required back-up area may be reduced by 2 feet. Parking provided in parking structures with more than one floor or with underground parking shall have a minimum parking space width of 8 feet and 6 inches, a minimum depth of 18 feet, and a back-up aisle with a minimum depth of 24 feet.
- B. **Aisle Width Dimensions for Open Parking Spaces.** The minimum width of 2-way aisles shall be 20 feet unless: (1) the aisle serves as principal driveway access to a parking lot which serves 50 or more spaces and/or (2) the driveway provides direct access to a public street. In those instances, the aisle shall be not less than 25 feet in width. Any drive aisle required by the California Fire Code to provide access to the building must be a minimum of 26 feet in width and located within 150 feet of the exterior of any point of the building unless otherwise approved by the Fire Marshal.

- C. **Large and Small.** The minimum dimensions of open parking spaces and travel aisles are as set forth in the Schedule 18.41.070-A. The dimensions in the schedule may be interpolated if the parking angle does not directly correspond to those listed. The requirements are illustrated on the drawings following the table:

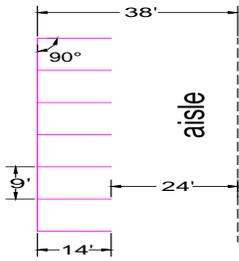
**Schedule 18.41.070-A: Parking Space and Aisle Dimensions**

Angle of Space	Distance from Interior of Space to Aisle	Aisle Width	Space Width	Space Length
Large Car Spaces				
0 degrees	10 feet	12 feet	10 feet	22 feet
30 degrees	17 feet 3 inches	12 feet	9 feet 6 inches	18 feet
45 degrees	19 feet 5 inches	13 feet	9 feet 6 inches	18 feet
60 degrees	20 feet 4 inches	17 feet	9 feet 6 inches	18 feet
90 degrees	18 feet	23 feet	10 feet	18 feet
90 degrees	18 feet	24 feet	9 feet 6 inches	18 feet
90 degrees	18 feet	28 feet	9 feet	18 feet
Small Car Spaces				
0 degrees	9 feet 6 inches	12 feet	9 feet	20 feet
30 degrees	14 feet 10 inches	12 feet	9 feet	14 feet
45 degrees	16 feet 3 inches	13 feet	9 feet	14 feet
60 degrees	16 feet 7 inches	15 feet	9 feet	14 feet
90 degrees	14 feet	23 feet	9 feet 6 inches	14 feet
90 degrees	14 feet	24 feet	9 feet	14 feet

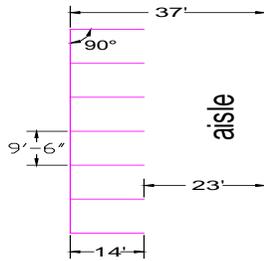
Notes:

1. Minimum parking space width is 9 feet, except where alongside a structure, pole post, wall, or fence, in which case the minimum width is 10 feet.
2. Locations of required parking spaces, garage spaces, garages and carports, are also governed by Official Plan Lines, and other regulations of this chapter.
3. Aisle widths may be interpolated where parking spaces are designed with a width between those specified above (e.g., a space width of 9 feet 9 inches would allow an aisle width of 23 feet 6 inches).

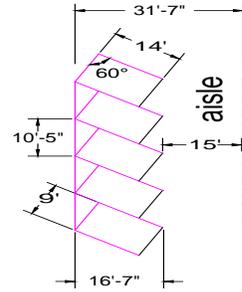
**COMPACT VEHICLE PARKING**



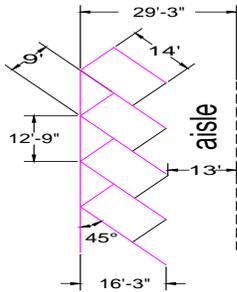
**90° Parking (9')**



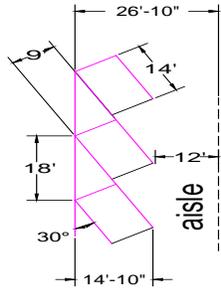
**90° Parking (9'-6")**



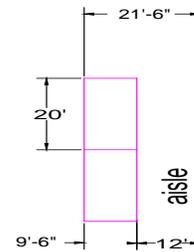
**60° Parking**



**45° Parking**



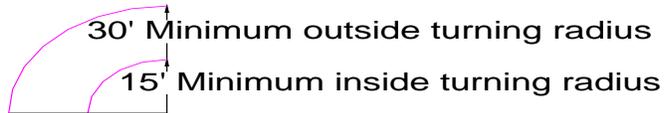
**30° Parking**



**Parallel Parking**

(45 feet for fire lane)

(29.5 feet for fire lanes)



**Minimum Turning Radius**

NOTE 1 - Minimum compact parking-space width is 9 feet, except where alongside a structure, pole, post, wall, or fence, in which case the minimum width is 10 feet. The minimum parking-space length is 14 feet.

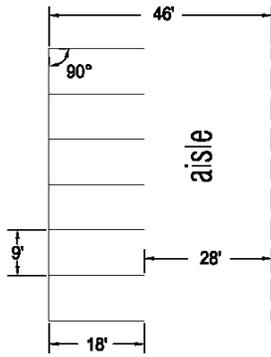
NOTE 2 - Locations of required parking spaces, garage space, garages, and carports are also governed by Official Plan Lines and other regulations of this chapter.

NOTE 3 - Access to fire lanes and the lanes themselves shall have a minimum inside turning radius of 29.5 feet and an outside turning radius of 45 feet.

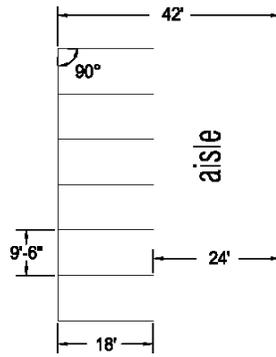
NOTE 4 - Required fire access lanes shall have a minimum width of 26 feet unless otherwise approved by the Fire Marshal.

\*Aisle widths may be interpolated where parking spaces are designed with a width between those specified above.

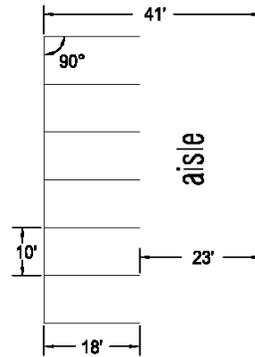
### STANDARD VEHICLE PARKING



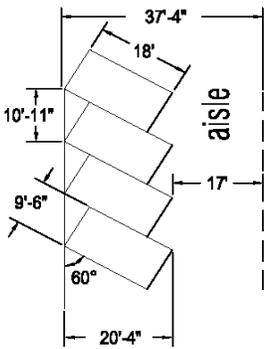
90° Parking (9')



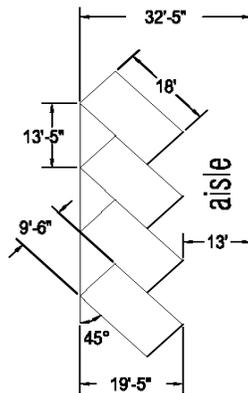
90° Parking (9'-6'')



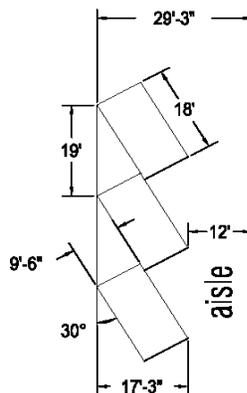
90° Parking (10')



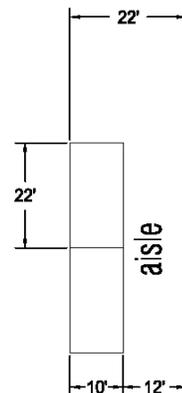
60° Parking



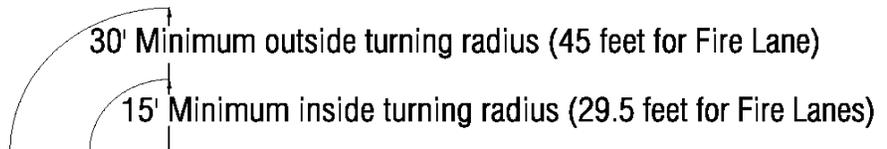
45° Parking



30° Parking



Parallel Parking



### Minimum Turning Radius

NOTE 1- Minimum parking-space width is 9 feet, except where alongside a structure, pole, post, wall, or fence, in which case the minimum width is 10 feet. The minimum parking-space length is 18 feet.

NOTE 2- Locations of required parking spaces, garage space, garages, and carports are also governed by Official Plan Lines, and other regulations of this chapter.

NOTE 3- Access to Fire Lanes and the lanes themselves shall have a minimum inside turning radius of 29.5 feet and an outside turning radius of 45 feet.

NOTE 4- Required fire access lanes shall have a minimum width of 26 feet unless otherwise approved by the Fire Marshall.

\* Aisle widths may be interpolated where parking spaces are designed with a width between those specified above (e.g., a space of 9 feet 9 inches would allow an aisle width of 23 feet 6 inches).

#### D. Reductions in Space Length/Aisle Width

Where there is landscape or a nonpublic sidewalk with a minimum width of 6 feet at the front end of a parking space, the required length of the parking space may be reduced by 2 feet except for handicap spaces [see Section 18.41.170(D)]. However, where there is landscape or a nonpublic sidewalk with parking on both sides, the landscape area or sidewalk is to be a minimum of 8 feet in width in order to allow a 2-foot reduction in parking space length on both sides. Where there is landscape or a nonpublic sidewalk with a minimum width of 6 feet located at the rear of a back-up area serving 90-degree parking, the back-up aisle may be reduced 2 feet in width.

#### 18.41.080 Surfacing

Off-street parking spaces, access points, aisles, driveways, and travel ways shall be paved with asphalt concrete or Portland cement concrete. All paved areas shall be properly and continuously maintained in safe condition while serving the use of the property. Maintenance shall include crack sealing, chip or slurry sealing, patching, or asphalt overlay as needed to prevent cracking, holes, or surface deterioration. Upon approval of the Director or his/her designee, retail and industrial uses may pave 10 percent of the required parking spaces in turf block or equivalent permeable paving materials. All areas within the parking area that are not required to be surfaced shall be landscaped.

Parking and access ways may be developed with gravel only in conjunction with the following uses by site development permit:

- A. **Outside Storage or Display for Commercial or Industrial Purposes.** A 50-foot-long asphalt or concrete driveway shall be provided from the paved street or back of sidewalk onto the property to prevent gravel or dirt from being carried onto the public street. Storage areas shall at a minimum, be surfaced with a gravel or 4-inch-thick road base with dust palliative to reduce particulate matter. Unless otherwise specified by the Fire Marshal, travel aisles shall be minimally surfaced with a 4-inch-thick road base with dust palliative to support emergency-service vehicles and reduce particulate matter. Exceptions to the surfacing requirements for temporary uses may be allowed by the Development Services Director.
- B. **Residential Driveways.** Where necessary to provide access to required parking spaces, driveways with a length of 150 feet or less, proposed off an improved public street for single-family houses, shall be improved for their entire length with asphalt, concrete, or a chip and seal surface. For residential driveways in excess of 150 feet in length, the Director may waive the requirement for paving by zoning exception provided that: (1) the first 30 feet from the paved street or back of sidewalk shall be paved, (2) the driveway-surfacing requirements of the Fire Marshal shall be met, and (3) the driveway does not lie within 75 feet of an existing or future residence. For residential driveways that do not have access to

an improved public or private street, surfacing shall be in accordance with the requirements of the Fire Marshal.

- C. **Paving of Alleys.** Where off-street parking proposes to use public alleys for access or back-up lanes, said alleys shall be improved to the requirement of the City Engineer as if it were part of the required off-street parking area.

### **18.41.090 Lighting**

Off-street parking areas for nonresidential uses providing parking spaces for use by the general public shall be provided with a maintained minimum of one footcandle of light on the parking surface from dusk until the termination of business every operating day. Such lighting, which would cause unreasonable annoyance to occupants of the neighboring properties or otherwise interfere with the public health, safety, or welfare, shall be so arranged as to reflect light and glare away from adjoining premises and streets.

Ground-mounted light poles serving parking areas shall be located within a planter or incorporated into a walkway or other pedestrian area. A ground-mounted light pole with a concrete pedestal greater than 6 inches above grade, which is not screened by vegetation, shall incorporate pedestal design enhancements (e.g., raised relief, textured, exposed aggregate, or like treatment).

### **18.41.100 Screening and Landscape**

- A. **Fencing.** In order to reduce or eliminate headlight glare and loss of privacy, newly constructed outdoor off-street parking areas that cause vehicle headlights to be cast upon a neighboring residential window or a vacant lot in a residential district shall be screened with a masonry wall or a solid wooden fence not less than 4 feet in height. Said screening shall be maintained in good condition at all times, shall be kept free at all times of advertising signs, and shall be set back from any abutting public street a distance equal to the distance which such parking area must be set back from any abutting public street.

No wall shall be required where there exists a wall that complies with the requirements of this paragraph, either on the common property line or adjacent thereto, on either side of the common property line, provided, however, that if such wall is thereafter removed, a replacement wall shall be required forthwith from the owner of the parking facility.

Where a wood fence is substituted for a solid masonry wall, such fence shall be constructed so that the boards overlap or are otherwise designed such that a person cannot see through them as a result of subsequent shrinkage. The fence shall have fence posts of either wood treated to resist rot and termites or of iron, steel, or masonry; and in all cases, fence posts must be set in concrete. Under no circumstances shall walls, fences, or shrubbery be placed or maintained on the property in such a manner as to interfere with visibility so as to

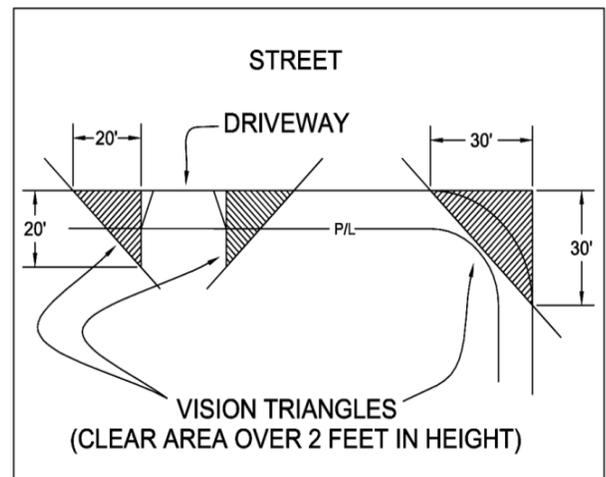
endanger safe ingress and egress. As an alternative to a fence, an earth berm, or earth berm and wall, can be substituted for the required fence.

- B. **Street-Side Planter.** The required front and corner side-yard setbacks shall be landscaped. Notwithstanding the setback requirements established by this ordinance, where a parking lot abuts a 2-lane public street, it shall be separated therefrom by a planter not less than 10 feet in width. Where a parking lot abuts a 4-lane-or-more public street or a street designated on the General Plan as a 4-lane-or-more arterial street, it shall be separated therefrom by a planter not less than 15 feet in width. In commercial districts, landscaped setbacks from non-access State highways shall average no less than 20 feet (15 feet minimum). Planter-width requirements are as measured from the property line. Planter-width minimums can be met using variable widths and may encroach into setback areas provided that the average width meets the minimum-width requirements as determined by the Development Services Director. If the sidewalk on the street is not adjacent to the property line, the area between the back of the sidewalk and the property line shall also be landscaped in addition to the above requirements. Public sidewalks may be located within the on-site landscape areas. Wherever the Zoning Code or more restrictive requirement of the City requires a greater-width planter than the above minimum, the greater requirement shall prevail.

The landscape planter between the parking facility and the street shall include shrubs that will attain a minimum height of 3 feet above parking lot grade within 18 months of installation. The plant materials and spacing shall be adequate to effectively screen the parking facility from the street. City-approved street trees shall be planted within the street-side planter on 30-foot centers or in groupings approved by the Development Services Director.

As an alternative to plants alone, the following other techniques may be used:

1. Grass-covered berm a minimum of 3 feet in height with a slope not steeper than 3:1.
2. A 3-foot-high, open decorative fence in combination with climbing and nonclimbing plants.
3. A 3-foot-high, minimum 4-inch-thick, decorative solid masonry or concrete wall at the back of the planter with a ground cover and/or other plants between the wall and the street.
4. Vegetation or screening of any type shall not exceed 2 feet in height within areas where adequate vehicle sight distance would otherwise be obstructed. Where such



screening is adjacent to a street corner or driveway intersection, screening shall not exceed 2 feet in height in the triangle formed by the corner and points at the curb 30 feet from the intersection or similar corner points within 20 feet of a driveway intersection.

- C. **Shade Trees.** Shade trees shall be planted in the parking lot at a ratio of one tree for every 4 spaces. Trees shall be dispersed on a generally equal basis throughout the parking lot to maximize the shading effect on the parking stalls. The landscape planter providing for any required tree shall have a minimum area of seventy-two- (72) square-feet and a minimum width of six feet (6'). To qualify as parking lot shade trees, the tree must be of a species and variety with an ultimate height and canopy that will provide a shading effect, and the trees must be within seven and one half (7.5') feet of a parking space or driveway aisle and must not be located within the public right-of-way. A parking space shall not be more than fifty feet (50') from a shade tree. The planting plans shall be submitted with the building-permit application and shall illustrate how the trees are to be irrigated and protected. The minimum size of each tree to be planted shall be a 15-gallon size. In those instances where parking is proposed underground or within a building, the tree to parking space ratio shall be 1:10. Said trees shall be planted within street-frontage planters or within other landscaped areas in the development.
  
- D. **Parking Lot Interior Landscape.** The interior of all parking lots is to be landscaped at a ratio of 60 square feet of landscape for each required parking space. For each additional space provided that exceeds the minimum parking requirement by more than five (5) spaces, or ten percent (10%), whichever is greater, eighty- (80) square-feet of landscape shall be provided for each excess space. The total required interior landscape area shall be evenly distributed throughout the parking areas. This requirement does not apply to parking lots that are underground or within buildings. Landscape areas that may be applied towards the required parking lot interior landscape area must have a minimum dimension of 4 feet, be surrounded by or within seven-and-one-half-feet (7.5') of a parking space or the aisle or pedestrian walkway serving it, or be within the required streetside landscape area adjacent to a parking space or aisle serving it. Landscape within a public right-of-way or in excess of the required streetside landscape addressed in Subsection B does not count toward meeting the minimum requirement. No more than twelve (12) parking spaces may be constructed in a row without separating the spaces with a landscape planter a minimum of 8 feet in width. Rows of parking spaces shall be separated from adjoining driveways with landscape end islands or peninsulas that are a minimum width of eight feet (8'). When calculating landscape area or width minimums, the area of the protective curbing shall not be included.
  
- E. **Pavement Edge and Planter Protection.** Landscaped areas and pavement edges in all multiple-family, commercial, and industrial zones shall be protected from damage and deterioration by the placement of 6-inch-high, securely anchored, continuous concrete curbs or equivalent materials which have a minimum width of 6 inches.

- F. **Building Separators.** Excepting single-family residences and second dwelling units, parking spaces directly abutting a building are prohibited. Parking areas shall be separated from a building by a raised walkway or landscape at least 4 feet in width.

#### **18.41.110 Pedestrian Access**

Office and retail developments which have parking lots deeper than 95 feet at the main entrance of a building and have more than 100 parking spaces shall include in the site design pedestrian walkways a minimum of 4 feet in width which provide safe travel between parking areas and the building(s) and from the building(s) to adjacent buildings and land uses within the same multiple-building complex. A walkway shall be distinctive in design to set it apart from the parking lot pavement areas.

#### **18.41.120 Drainage**

Off-street parking facilities shall be designed such that drainage is carried away from structures and does not flow over public sidewalks or within vehicle unloading areas.

#### **18.41.130 Shopping Cart Bays**

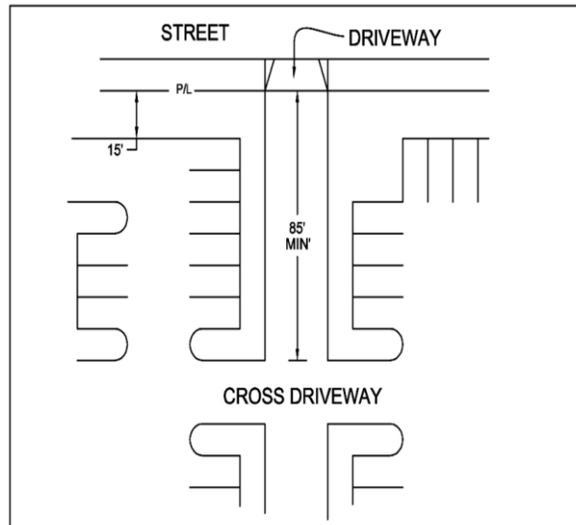
Parking lots that provide collection areas for shopping carts shall incorporate a low-profile design for the cart-return bays (i.e., raised curb channel or metal railings less than one foot in height). If carts are stored outside during non-operating hours, shopping carts shall be locked during non-operating hours.

#### **18.41.140 Curb Cuts**

**The installation of curb cuts shall be in accordance with the following standards:**

- A. Curb cuts or use of rolled curb for driveway purposes may not exceed 50 percent of the lot width along any street and shall be limited to a width of forty-feet- (40') each, except for industrial uses where a width of fifty feet (50') (which includes curb-return radii and driveway throat-width totals added together) may be permitted for truck entrances. Greater widths may be permitted subject to obtaining a zoning exception.
- B. Curb cuts in all zones, except single-family, shall be limited to one cut per street frontage and, in commercial zones, shall not be closer than twenty feet (20') from the existing or projected curb return. In no case shall a curb return have a radius less than twenty feet (20'). By site development permit, the Development Services Director may allow additional curb cuts where consideration of a specific use or site size or configuration indicates that additional access is required and said request will not unduly restrict the street capacity or reduce traffic safety. In single-family areas, a curb cut shall not be closer than ten feet (10') from the existing or projected curb return unless a zoning exception is first obtained.

- C. As measured from the top of the curb, commercial curb cuts for 2-way driveways serving property shall be a minimum of 35 feet in width or as approved by the City Engineer.
- D. Primary driveway entrances to a facility that has in excess of 150 parking spaces shall be protected from on-site cross traffic by berms, medians, or planters a minimum distance of 85 feet behind the property line to allow stacking of cars exiting onto a public street and movement of cars off the public street.



- E. Whenever a change of use, construction of a new building, or redesign of a parking lot removes the need for an existing driveway or curb cut, it is the duty of the property owner to fill in the driveway and curb-cut areas to conform to adjacent sidewalk and curb improvements.
- F. Whenever curb, gutter, sidewalk, or driveway are to be constructed or removed, an encroachment permit shall be obtained from the Transportation and Engineering Director pursuant to Section 13.16.010.
- G. Driveway curb cuts shall not be located at points where they are likely to cause traffic conflicts. Site plans shall be provided that depict surrounding driveways and street design.

### **18.41.150 Parking Space Setback from<sup>\*</sup> Arterial Driveway Entrances**

To prevent vehicle backing movements in parking lots from conflicting with street traffic flow, 90-degree angled, off-street parking spaces adjacent to a 4-lane arterial, which use the entrance driveway as backup area, shall maintain a minimum setback of 20 feet from the street-side property line. Similarly, parking spaces with less than a 90-degree angle, which result in vehicles

backing toward the street, shall maintain a 25-foot setback from the street-side property line. Where the street does not have a parking lane, an additional 5-foot setback shall be required. Open areas between the parking spaces and the street shall be landscaped.

**18.41.160 Reserved**

**18.41.170 Access Requirements for the Disabled**

The following special parking requirements are applicable to all commercial and industrial land uses and certain multiple-family residential land uses. These special stalls shall be in the closest proximity to the facility for which they are designated in order to encourage their use.

- A. **Parking Access for the Disabled.** Parking spaces for the physically disabled shall be provided in accordance with the following provisions and as mandated by the California State Accessibility Standards (California Building Code, Chapter 11) and the Council of American Building Officials (CABO)/American National Standards Institute (ANSI) A117.1-1992, which is a part of this code. Certain multiple-family developments are exempt from the requirement to provide parking for the physically disabled. For determination of which multiple-family projects may be exempt from these requirements, refer to Chapter 11 of the California Building Code.

1. ***Spaces Required.***

- a. For commercial, industrial, and publicly funded residential projects, the number of spaces required to be accessible for persons with physical disabilities shall be proportional to the total number of parking spaces provided for each type of project. (Schedule 18.41.170-A establishes the minimum number of accessible spaces required.)

**Schedule 18.41.170-A: Minimum Number of Accessible Spaces for Disabled**

Total Number of Spaces Required	Minimum Required Number of Accessible Spaces <sup>1</sup>
1-25	1
26-50	2
51-75	3
76-100	4
101-150	5
151-200	6
201-300	7
301-400	8
401-500	9
501-1000	2 percent of total
1001 and over	20 plus 1 per 100 above 1000

Note:

<sup>1</sup> At least 1 accessible space must be "van accessible." One van-accessible space shall be provided for each 6 accessible spaces or fraction of.

- b. If assigned parking is provided at privately funded multiple-family building sites, designated accessible parking at the dwelling unit shall be provided on request of residents with physical disabilities on the same terms and with the full range of choices (e.g., surface parking or garage) provided for other residents, with accessible parking on a route accessible to wheelchairs for at least 2 percent of the covered dwelling units. Signage is not required. When visitor parking is provided, a minimum of 5 percent of parking shall provide access to grade-level entrances of multiple-family dwellings and accessible parking at facilities (e.g., swimming pools) that serve accessible buildings. Visitor parking spaces shall be provided with signage. Such sign shall not be blocked from view by a vehicle parked in the space.
- c. **Medical Care Outpatient Facilities.** At facilities providing medical care and other services for persons with mobility impairments, parking spaces complying with this section shall be provided in accordance with Table 3, except as follows:
  - (1) **Outpatient Units and Facilities.** Ten percent of the total number of parking spaces provided serve each such outpatient unit or facility.
  - (2) **Units and Facilities that Specialize in Treatment or Services for Persons with Mobility Impairments.** Twenty percent of the total number of parking spaces provided serve each such unit or facility.

- B. **Accessible Parking Required.** Accessible parking spaces shall be located as near as practical to a primary building entrance. In facilities with multiple accessible building entrances with adjacent parking, accessible parking spaces shall be dispersed and located closest to the accessible entrances. When practical, the accessible route of travel shall not cross lanes for vehicular traffic. When necessary, the route of travel shall be designated and marked as a crosswalk.
- C. **Identification of Spaces.** Signage and striping shall conform to the requirements of Chapter 11 of the California Building Code.
- D. **Accessibility.** One in every 8 accessible spaces, but not less than one, shall be "van-accessible," consisting of a 12-foot-wide parking space and a 5-foot-wide access aisle, or alternatively, a 9-foot-wide parking space, together with an 8-foot-wide access aisle, in accordance with the accessibility requirements of the California Building Code. Where single spaces are provided in addition to the van-accessible space, they shall be 14 feet wide and outlined to provide a 9-foot-wide parking area and a 5-foot-wide loading and unloading area on the passenger side of the vehicle. When more than one space is provided, 2 spaces can be provided within a 23-foot-wide area lined to provide a 9-foot-wide parking area on each side of a 5-foot-wide loading and unloading aisle in the center. The minimum length of each parking space shall be in accordance with Section 18.41.070, not to be less than 18 feet.
- E. **Arrangement of Accessible Parking Spaces.** In each parking area, a bumper or curb shall be provided and located to prevent encroachment of cars over the required width of walkways. The space shall be so located that people with physical disabilities are not compelled to wheel or walk behind parked cars other than their own. Pedestrian ways that are accessible to the physically disabled shall be provided from each such parking space to related facilities, including curb cuts or ramps as needed. Ramps shall not encroach into any parking space.
- F. **Slope of Parking Space.** Surface slopes of reserved parking spaces, a 4-foot-wide area to the sides and rear of the spaces, and access aisles for people with physical disabilities shall be the minimum slope possible and shall not exceed one unit vertical to 50 units horizontal (2 percent) in any direction.

#### **18.41.180 Driveway Grades/Widths**

- A. **Grades.** No driveway or portion thereof shall have a slope exceeding 15 percent, except that within a public right-of-way, the slope shall not exceed 2 percent in the area required for pedestrian path of travel nor 8 percent between the path of travel and the right-of-way line. The slope shall be measured at the centerline of the driveway. Driveways greater than 150 feet in length shall not exceed a grade of 12 percent unless fire-safety measures as may be required for the structure by the Fire Marshal are installed, but in no case shall the grade exceed 15 percent.

**B. Widths.**

1. Driveways, excluding areas used for back-up aisles, shall be provided as follows:
  - a. Where 6 or fewer parking spaces are required for residential parking areas and the structure(s) are located within 150 feet of a public street, the minimum driveway width shall be 12 feet except as noted in "C" below.
  - b. Where parking spaces are required for residential parking areas and the structure(s) are located at a greater distance than 150 feet from a public street, the minimum driveway width shall be 20 feet (16 feet of paving and 2-foot-wide graveled shoulders) or as approved by the Fire Marshal. A turnaround area for emergency-response vehicles shall be provided and shall be designed in accordance with the specifications provided by the Fire Marshal.
  - c. In single-family residential districts, driveways which provide access to covered parking located within 150 feet and at the rear of a main structure shall have a minimum width of 10 feet.
2. All other driveways shall have a minimum width of 12 feet for one-way traffic and 20 feet for 2-way traffic except in those instances where a greater width is required by Section 18.41.180(B)(3). One-way driveways shall be clearly delineated. Additional driveway width may be required to provide for transition to a wider drive opening at the street right-of-way line, to accommodate turn lanes, to accommodate the projected traffic volume, or to otherwise provide safe and convenient accessibility to parking spaces.
3. Driveways providing access to open parking areas taking access from one or both sides shall be a minimum of 23 feet in width, or as otherwise allowed in Schedule 18.41.070-B and Section 18.41.180(B)(1).

**18.41.190 Setback From Streets and Alleys**

Where a garage or carport is directly accessible from a public street, it shall have a minimum setback of 20 feet. Where a garage or carport is directly accessible from an alley, it shall have a minimum setback of 5 feet.

Notwithstanding any requirements of this code, in cases where the elevation of the lot at a point 20 feet—as measured from the back of curb along the entire frontage of the lot—is 7 feet above or below the level of the curb, a private garage for a single-family residence may be built (attached or detached) to within 10 feet of the front property line of the lot provided that a zoning exception shall be obtained in each case and that a third open parking space is provided adjacent to the covered parking.

### 18.41.200 Additional Standards

- A. **Locations of Residential Parking.** All required, covered, off-street parking spaces shall be located conveniently accessible to the dwelling unit served by such parking space.
- B. **Tandem Parking.** Tandem parking or parking where a car or cars have to be moved in order to allow a car to back from a parking space, counts only as one parking space. To meet off-street parking requirements for more than one space, each car must be able to enter and exit a parking space independent of the movement of any other vehicle.
- C. **Drop-Off Points.** When located outside the Downtown District, parking areas for public assembly or institutional facilities listed in this section shall include a designated on-site location for dropping off passengers at an entrance to the facility in advance of parking the vehicle. Drop-off areas are to consist of vehicle turnout lanes located outside normal travel lanes. Drop-off points are to be provided for hotels and motels, schools with 50 or more students, churches with a capacity of 100 or more, public transportation terminals, places of public assembly, and public buildings.
- D. **Slope.** The finished grade of a parking lot shall not exceed 5 percent on an angle of the parked car that would allow the car to roll from the parked position.
- E. **Vertical Clearance.** Except for residential uses, covered parking is to have a vertical clearance of at least 7 feet 6 inches above the finished parking lot surface. Where a building or sign extends over a parking area, the minimum clearance shall be 10 feet for auto and 15 feet for trucks.
- F. **Parking Structures.** The exterior elevations of parking structures shall be designed to minimize the use of blank concrete facades. Textured concrete, pilasters, planters or trellises, or other architectural treatments shall be provided to accomplish this requirement. Perimeter landscape shall conform to Section 18.41.100 of this chapter. Additionally, irrigated City-approved street trees shall be planted on 30-foot centers in the required perimeter landscape. Architectural and landscape plans shall be subject to the approval of the Director.

### 18.41.210 Off-Street Loading Spaces Required

A building, or part thereof, having a floor area of 10,000 square feet or more that is to be occupied by a manufacturing plant, storage facility, warehouse facility, goods-display facility, retail store, wholesale store, market, hotel, hospital, mortuary, laundry, dry-cleaning establishment, or other use or uses similarly requiring the receipt or distribution by vehicles or trucks of material or merchandise shall provide at least one off-street loading space, plus one additional such loading space for each 40,000 square feet of floor area. Such off-street loading space shall be maintained during the existence of the building or use it is required to serve. Truck-maneuvering areas shall not encroach into required parking areas, travel-ways, or street rights-of-way.